



## **NOTICE OF PUBLIC MEETING – Street Committee**

The City of Adel Street Committee will meet at Adel City Hall, 301 S. 10<sup>th</sup> Street Adel, Iowa, in the Council Chambers on Tuesday, August 7, 2018 at 6:00 p.m.

### **AGENDA**

1. Call to Order
2. Approval of Minutes – July 9, 2018
3. Updates on Adel Growth & Safety Transportation Project – Southside
  - a. HWY 169 Improvements & Turning Lanes
  - b. Trails/Sidewalks & Underpass
  - c. Meadow Road Surfacing
  - d. Any Schedule Updates? Future Public Meeting?
  - e. Consider Focus Group / Steering Committee
4. Review of McClure Engineering Estimates for Potential Overlay Projects (Unused 2017 Main Street Bond Proceeds)
5. Dallas County Parking Lot Rehabilitation – Consider No Parking on North 10<sup>th</sup> Street between Court and Rapids
6. Any Other Business
7. Adjournment

8/6/2018 3:08:48 PM

*Please Note: Members or a quorum of members of other City Boards, Commissions, Committees, and / or the Council may be in attendance. Only items on the agenda may be acted upon and / or discussed.*



### **July 9, 2018 Street Committee – Minutes**

**The City of Adel's Street Committee met at Adel City Hall, 301 S. 10<sup>th</sup> Street, Adel, Iowa, in the Council Chambers on July 9, 2018 at 6:00 p.m.**

The Street Committee meeting was called to order at 6:00 p.m. by Chair Rob Christensen. Members present: Christensen and Selby. Others in attendance: Council members McAdon and Ockerman, City Administrator Brown, Public Works Director Overton, Finance Director Sandquist, McClure Engineering rep. Schug, and some residents.

Selby motioned, seconded by Christensen, to approve the May 1, 2018 minutes. Motion carried unanimously.

Discussion began on the Adel Growth & Safety Transportation Project, which includes widening HWY 169, trails, an underpass, and paving Meadow Road. Schug presented a preliminary layout. The layout showed a section of HWY 169 from Greenwood Hills Drive to 302<sup>nd</sup> Place. The preliminary concept included sections with a middle turn lane, medians, and right-hand turn lanes. The sidewalks and trails, which are on both sides, are connected by an underpass.

Schug noted that the City would be meeting with the DOT on July 12 to request additional funding (i.e., the DOT is already funding \$1.5 million). The DOT has indicated that it may do an overlay project on HWY 169 from De Soto to Adel. It could be beneficial to align these projects to get a better bid price, though it may cause a delay.

Schug stated that the revised preliminary cost estimate of \$5.783 million was higher due to the medians. This estimate will likely change. Schug noted that the medians would help slow speeds down. Schug noted that two additional public information meetings would be scheduled at a later date.

Discussion moved to the DOT Corridor Access Agreements. Schug noted that these proposed agreements, which dictate access onto HWY 6 and HWY 169, are being revised by the DOT. There is no timeline, but they are expected to be issued later this year. Schug noted that the City will need to decide on the R-16 realignment at that point.

Discussion moved to the unused 2017 Main Street Bond proceeds. Brown noted that approximately \$451,000 was unused and that it can be used on practically any infrastructure project. The unused funds appear to be due to the lower bids and lower final costs. Schug noted that the City also tried to maximize the USDA-RD funds. Christensen noted that the dollar amount may allow for overlay work on phases 1-3 from the 2017 Streets CIP. Two methods are available: traditional overlay or crack and seat. Schug stated the latter costs 20% more but lasts longer. The committee will review next month.

Discussion moved to the County's parking project. Overton stated that the County would be resurfacing the driver's license parking lot with porous pavers and installing angled parking on North 10<sup>th</sup> Street. The final site plan will be delivered soon. Ockerman suggested that the County close one parking entrance. The committee will review the final plans next month.

Discussion moved to the proposal of "No Parking" near 318 South 9<sup>th</sup> (i.e., the new bed and breakfast in the old "Big Blue" house). Overton stated that this section can become crowded with traffic turning off of Greene Street. Ockerman suggested that the "No Parking" area go to the Raccoon River Valley Trail (RRVT).

Selby motioned, seconded by Christensen, to recommend to council that "No Parking" be implemented on South 9<sup>th</sup> Street from Greene Street to the RRVT. Motion carried unanimously. Brown will send a letter to property owners.

Discussion moved to the recent curb house number painting that a private company offered residents. Christensen noted that the company's flyers looked like they came from or were endorsed by the City, though they were not. Christensen asked whether the City had any liability, though it is unclear. Brown will check with other cities on this general issue.

With no other business, the meeting was adjourned at approximately 6:50 p.m.

Respectfully submitted,  
Anthony Brown, City Administrator

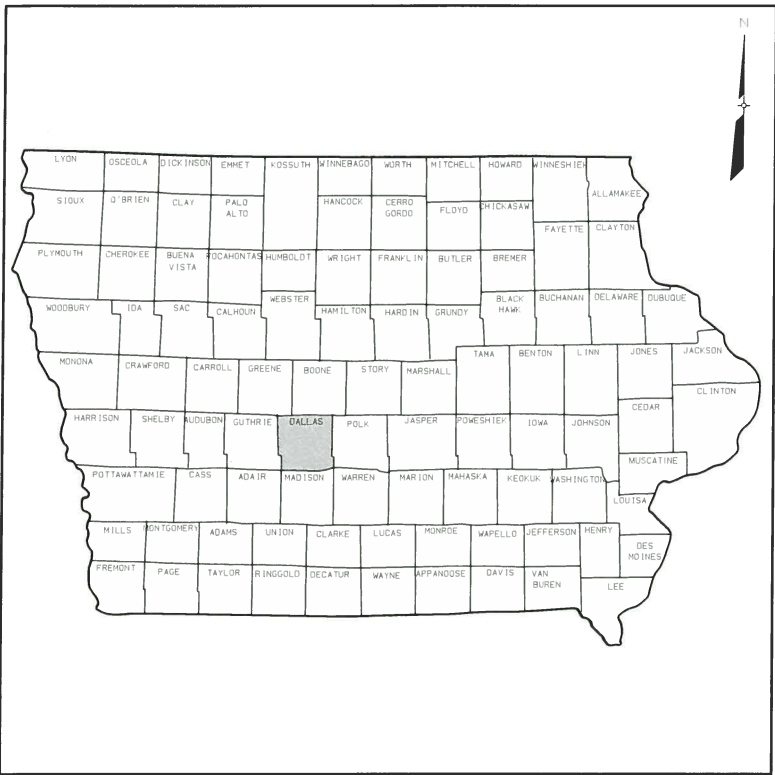
PARKING LOT REHABILITATION

PHASE 1 - DMV PARKING LOT &  
10th STREET PARKING

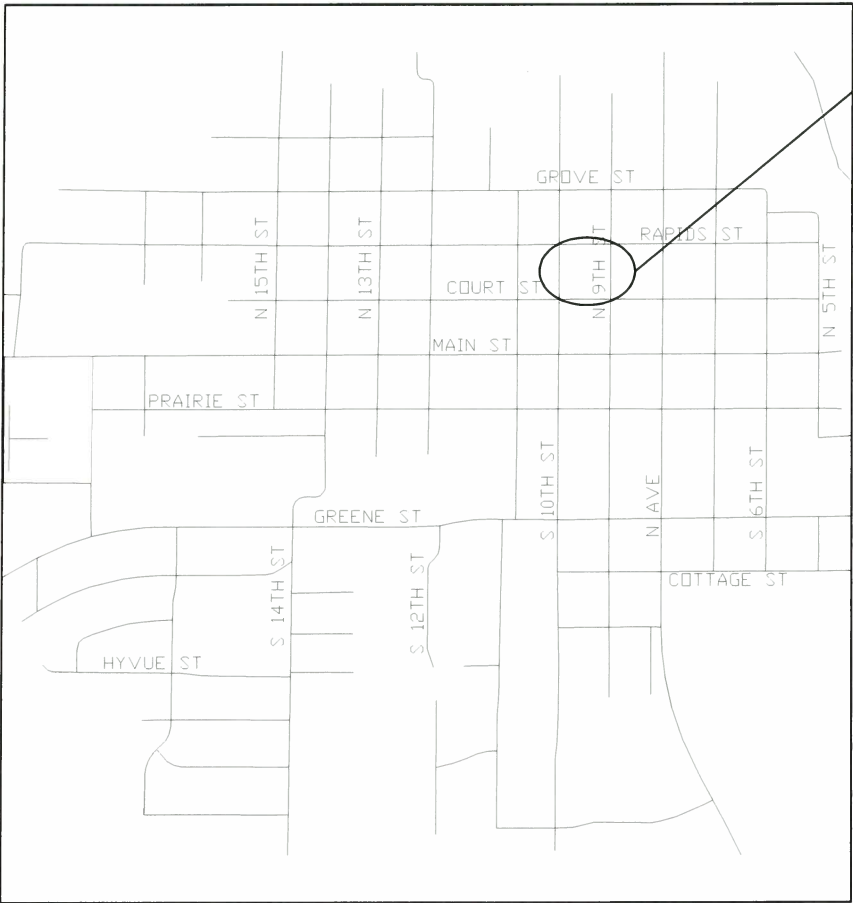
ADEL, IOWA

AUGUST 2018

SHEET NO	TITLE
1	TITLE SHEET
2	LEGEND & SYMBOLS
3	QUANTITIES AND ESTIMATE REFERENCE NOTES
4	GENERAL NOTES
5	STAGING NOTES
6-7	REMOVAL SHEETS
8-9	PROPOSED LAYOUT SHEETS
10	TYPICAL SECTION
11	JOINTING DETAILS
12	MISCELLANEOUS DETAILS
13-14	PROPOSED GRADING SHEETS
15-18	SIDEWALK SHEETS
19-20	PAVEMENT MARKINGS



VICINITY MAP  
NTS



LOCATION MAP  
NTS



PREPARED FOR **DALLAS COUNTY, IOWA**  
902 COURT STREET  
ADEL, IOWA 50003  
515-993-5806





























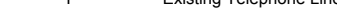








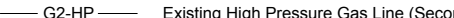

CHAIR

















APPROVED
BOARD OF SUPERVISORS

	I hereby certify that this Engineering document was prepared by me or under my direct personal supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Iowa.
	Todd L. Alllyn Date 8/3/18
	License number 14621
	My license renewal date is December 31, 2018

Pages or sheets covered by this seal:  
ALL SHEETS

PREPARED BY **AECOM**  
500 SW 7TH STREET, SUITE 301  
DES MOINES, IOWA 50309  
T 515.323.7910 F 319.232.0271











STANDARD SYMBOLS	
	Septic Tank
	Cistern
	L.P. Gas Tank (No Footing)
	Underground Storage Tank
	Latrine
	Luminaire
	Traffic Signal
	Traffic Signal with Luminaire
	TP Telephone Pedestal
	TVP Television Pedestal
	Telephone Pole
	Telephone Pole (Second Company)
	Telephone Pole (Third Company)
	Telephone Pole (Fourth Company)
	Telephone Pole (Fifth Company)
	Power Pole
	Power Pole (Second Company)
	Power Pole (Third Company)
	Power Pole (Fourth Company)
	Power Pole (Fifth Company)
	Electrical Highline Tower (Metal or Concrete)
	Telephone Riser Pole
	Power Riser Pole
	Telegraph Pole
	Satellite TV Dish
	Existing Water Line
	Existing Water Line (Second Company)
	Existing Sanitary Sewer Line
	Existing Telephone Line
	Existing Telephone Line (Second Company)
	Existing Fiber Optics Telephone Line
	Existing Storm Sewer Line
	Existing Gas Line
	Existing High Pressure Gas Line
	Existing Gas Line (Second Company)
	Existing High Pressure Gas Line (Second Company)
	Existing Power Line
	Existing Power Line (Second Company)
	Cable Television Line

	Guardrail (Beam or Cable)
	Guard Post (one or two)
	Guard Post (over two)
	Filler Pipe
	Gas Valve
	Water Valve
	Speed Limit Sign
	Mile Marker Post
	Sign
	Water Hook Up
	Radio Tower
	Tower Anchor
	Electric Box
	Traffic Signal Control Box
	Rail Road Signal Control Box
	Telephone Switch Box

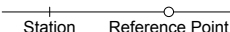

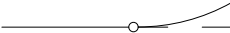






COMPANY	ADDRESS	REPRESENTATIVE	TELEPHONE
City of Adel	P.O. Box 248 Adel, Iowa 50003	Jordon Altenhofen	(515) 993-4525
CenturyLink		Tom Sturmer	(720) 578-8090
Dallas County	801 Court St. Adel, Iowa 50003	Cliff Gibson or Todd Noah	(515) 993-5818
MidAmerican Energy (Electric and Gas)	P.O. Box 657 Des Moines, Iowa 50306	Craig Ranfeld	(515) 252-6632
Mediacom		Paul May	(515) 246-2252

### RIGHT OF WAY LEGEND

	Proposed Right of Way
	Existing Right of Way
	Existing and Proposed Right of Way
	Easement and Existing Right of Way
	Borrow
	Easement (Temporary)
	Easement
	Excess
	Property Line
	Access Control

### CONVENTIONAL SIGNS

	Survey Line
	Section Corner
	Proposed Profile Grade
	Railroad
	Field Tile
	Culverts
	Stream

[illegible]



Contractor shall check for locations of utilities, drainage structures or other facilities in the construction area. Any damage to such facilities due to the contractor's carelessness shall be corrected at the contractor's expense.	
01-20-84 All holes resulting from operations of the contractor, including removal of guardrail posts, fence posts, utility poles, or foundation studies, shall be filled and consolidated to finished grade as directed by the engineer to prevent future settlement. The voids shall be filled as soon as practical - preferably the day created and not later than the following day. Any portion of the right-of-way or project limits (including borrow areas and operation sites) disturbed by any such operations shall be restored to an acceptable condition. This operation shall be considered incidental to other bid items in project.	204-2
10-29-02 It shall be the contractor's responsibility to provide waste areas or disposal sites for excess material (excavated material or broken concrete) which is not desirable to be incorporated into the work involved on this project. These areas shall not impact wetlands or "Waters Of The U.S." No payment for overhaul will be allowed for material hauled to these sites. No material shall be placed within the right-of-way, unless specifically stated in the plans.	213-1
10-27-98 The contractor shall apply necessary moisture to the construction area and haul roads to prevent the spread of dust. Refer to Article The cost of dust control shall be incidental to the project.	213-4
01-20-84 In order to avoid any unnecessary surface breaks or premature spalling, the contractor is cautioned to exercise extreme care when performing any of the necessary saw cutting operations for the proposed pavement removal.	221-4
01-20-84 The contractor shall not disturb desirable grass areas and desirable trees outside the construction limits. The contractor will not be permitted to park or service vehicles and equipment or use these areas for storage of materials. Storage, parking and service area(s) will be subject to the approval of the resident engineer.	232-5
09-27-94 The top 6 inches of the disturbed areas shall be free of rock and debris and shall be suitable for the establishment of vegetation, subject to the approval of the Engineer.	232-8
10-28-97 The contractor is expected to have materials, equipment, and labor available on a daily basis to install and maintain erosion control features on the project. This may involve seeding, silt fence, rock ditch checks, silt basins, or silt dikes.	232-10
10-29-02  BEFORE YOU DIG:  IOWA ONE CALL# 1-800-292-8989	262-3
01-93 All traffic control devices utilized shall be in accordance with the Manual on Uniform Traffic Control Devices and Section 8030 of the SUDAS Specifications.	ET-E.34

01-93	ET-E.37
All traffic control devices shall be furnished, erected, maintained and removed by the contractor.	
01-93	ET-F.1
Where public utility fixtures are shown as existing on the plans or encountered within the construction area, it shall be the responsibility of the Contractor to notify the owners of those utilities prior to the beginning of any construction. The Contractor shall afford access to these facilities for necessary modification of services. Underground facilities, structures and utilities have been plotted from available surveys and records, and therefore their locations must be considered approximate only. It is possible there may be others, the existence of which is presently not known or shown. It is the Contractor's responsibility to determine their existence and exact location and to avoid damage thereto. No claims for additional compensation will be allowed to the Contractor for any interference or delay caused by such work.	
01-93	ET-F.6
Contractors shall arrange with the various utility companies for support or relocation of any utilities and/or utility poles. Any charge for temporary supporting or relocating any utilities and/or utility poles shall be paid by the Contractor.	
01-93	ET-F.7
The information concerning underground utilities was compiled from information and sketches furnished by or obtained from utility companies and City records. The owner and the Engineer do not guarantee their accuracy. The Contractor is advised to determine the exact locations from the available sources of information or provide his own means of detection. The only case in which the Engineer will consider redesign or relocation of a proposed facility in the project is when an existing utility is located within the limit of two vertical lines located on the exterior of the proposed pipe or facility. In this case, the Engineer will try to have the utility relocated. If such a relocation is impossible, he will consider redesign or relocation of the proposed facilities. In both cases, Contractor shall be responsible for all underground utilities and shall not be separately compensated for delays or extra cost.	
01-93	ET-F.8
Prior to opening an excavation, effort shall be made to determine whether underground installations; i.e., sewer, water, fuel, electric lines, etc., will be encountered, and if so, where such underground installations are located. When the excavation approaches the approximate locations of such an installation, the exact locations shall be determined by careful probing or hand digging, and when it is uncovered, adequate protection shall be provided for the existing installation. All known owners of underground facilities in the area concerned shall be advised of proposed work at least 48 hours prior to the start of actual excavation.	
01-93	ET-G.6
The Contractor shall prepare a detailed schedule of work which shall be submitted at the preconstruction conference. The schedule shall show anticipated equipment and material deliveries and completion of major tasks in the project.	
01-93	ET-G.7
The general Contractor shall be responsible for the coordination of work between all equipment suppliers, crafts and subcontractors involved in the project including staging of construction and construction details.	

	R-12
<p>All trenches for storm sewer and for removal of existing storm sewer pipe and excavated areas adjacent to intakes and storm sewer manholes shall be backfilled and compacted to 98 percent of standard proctor density according to descriptions included in the special provisions for sanitary sewers. Compaction of backfill will not be measured or paid for separately but will be considered as incidental to the prices bid for the various sizes of storm sewer pipe, intakes, manholes and pipe removal.</p>	
	R-18
<p>Contractor shall protect all utilities. Contractor shall protect all storm sewer inlets from siltation and debris during construction.</p>	
<p>Finishing of border areas beyond curbs shall be considered incidental to other work.</p>	
<p>Unless otherwise directed or authorized, all asphaltic cement concrete and other bituminous materials which are not specifically addressed or described in the plans shall become the property of the contractor.</p>	
<p>Any saw cutting noted on the plans shall be construed to be incidental to other contract items.</p>	
<p>The location for storage of equipment by the contractor during nonworking hours shall be as approved by the City of Adel and Dallas County.</p>	
<p>Project shall be constructed according to 2018 version of SUDAS Specifications unless otherwise noted.</p>	

[illegible]

## STAGING NOTES

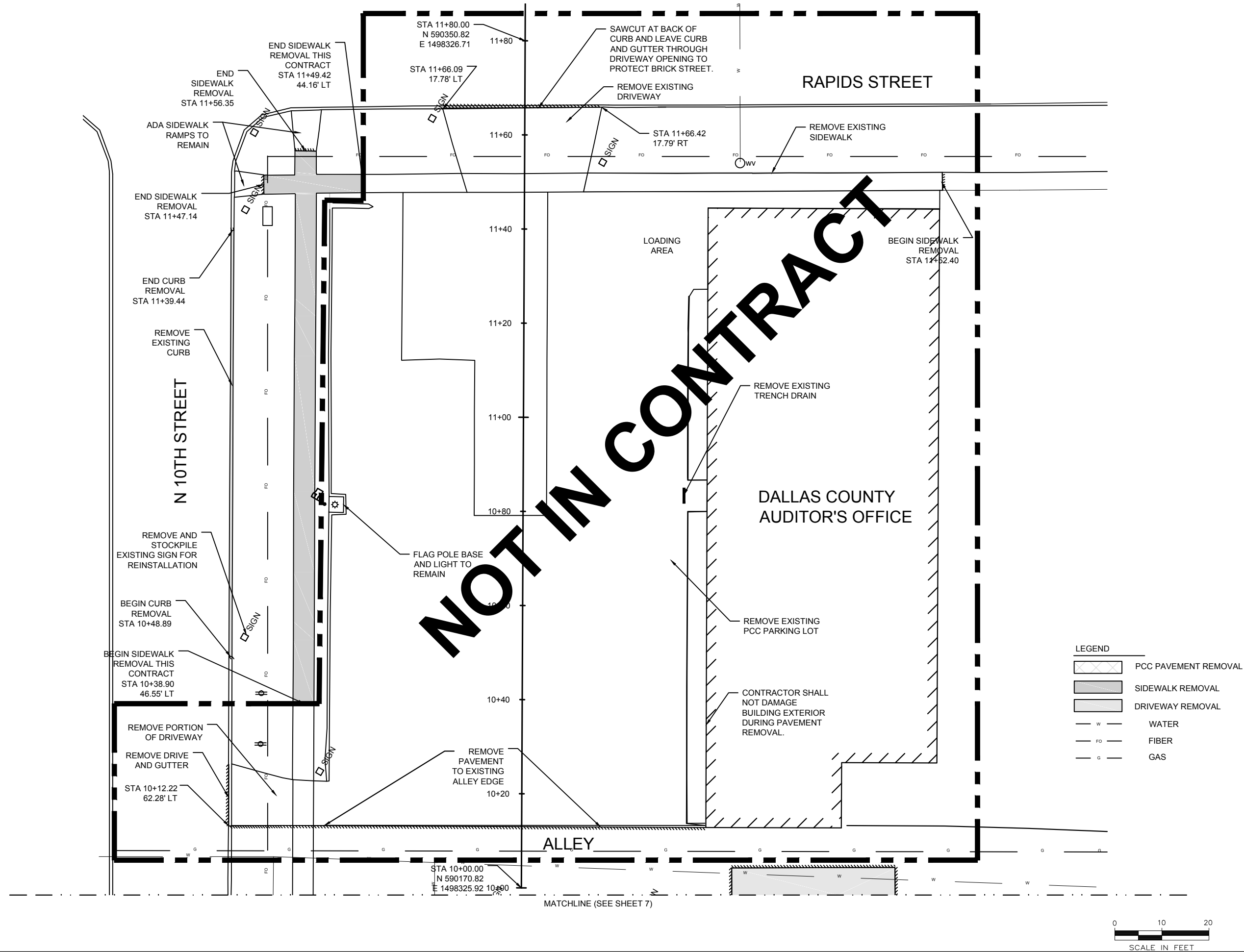
### STAGE 1 - ADDITIONAL STREET PARKING

1. SHIFT TRAFFIC ON N. 10TH STREET TO THE WEST UTILIZING CURRENT PARKING LANE. CHANNELIZERS SHALL BE UTILIZED TO ALLOW FOR TWO-WAY TRAFFIC ADJACENT TO PROPOSED STREET PARKING.
2. COMPLETE REMOVALS IN NEW STREET PARKING AND SIDEWALK AREA AND PREPARE SITE FOR CONSTRUCTION.
3. EXCAVATE AND INSTALL MODIFIED SUBBASE AND NEW PARKING AND SIDEWALK PAVEMENT AS SHOWN IN THE PLANS.
4. TOPSOIL AND SOD THE AREAS AT THE NORTH AND SOUTH END OF THE PARKING AREA AS SHOWN ON THE PLANS.
5. STAGE 1 MAY BE CONSTRUCTED CONCURRENTLY WITH STAGE 2.

STAGE 2 - DMV PARKING LOT

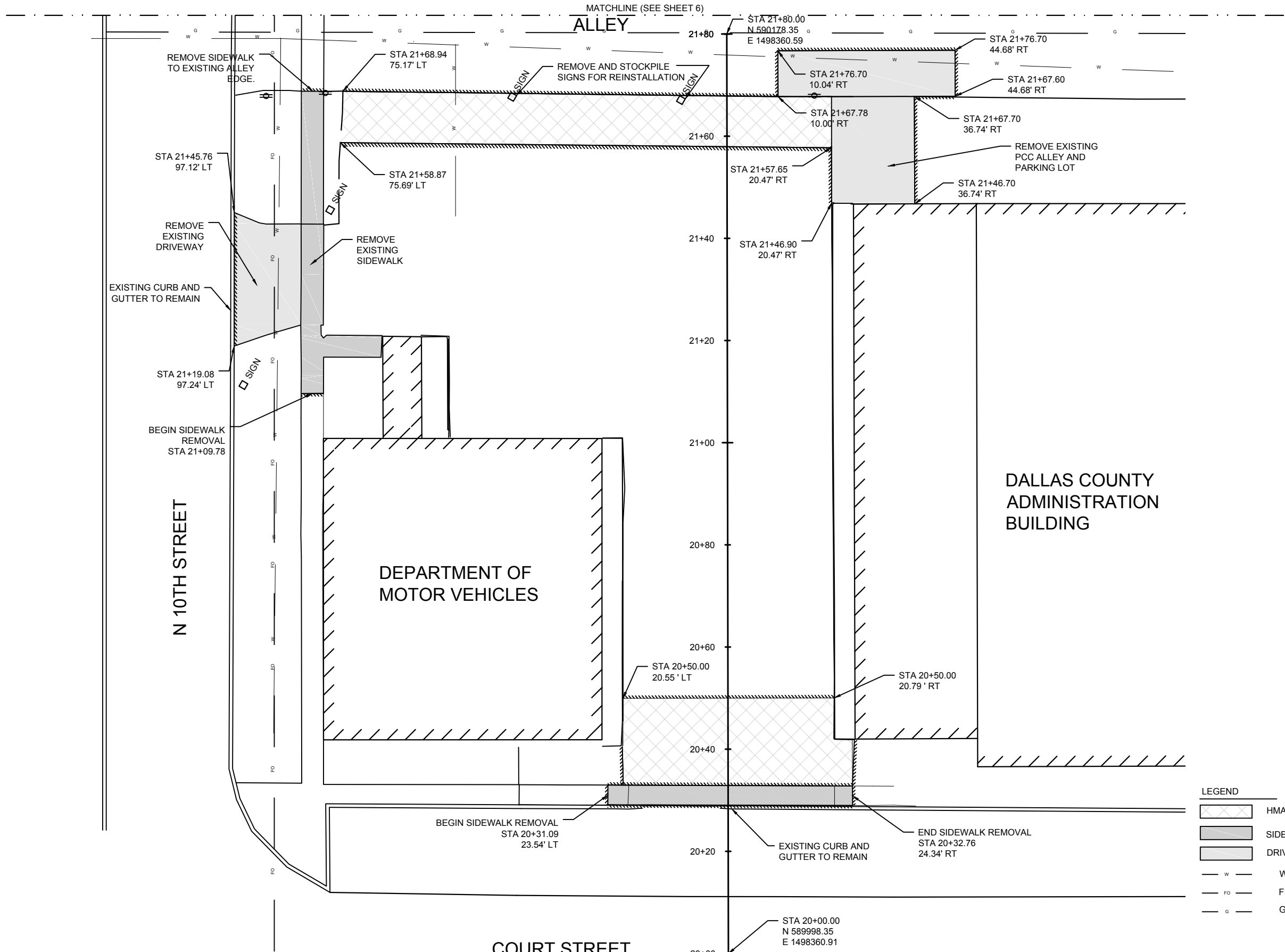
1. CLOSE DRIVEWAYS FROM COURT STREET, N. 10TH STREET AND THE ALLEY TO THE DEPARTMENT OF MOTOR VEHICLES PARKING LOT.
2. CLOSE SIDEWALK ON NORTH SIDE OF COURT STREET AND EAST SIDE OF N. 10TH STREET AND DETOUR PEDESTRIAN TRAFFIC TO OTHER SIDE OF STREETS.
3. COMPLETE REMOVALS IN SIDEWALKS AND PARKING LOT AND PREPARE FOR CONSTRUCTION.
4. INSTALL NEW SIDEWALK, DRIVEWAYS AND HMA OVERLAY.
5. TOPSOIL AND SOD DISTURBED AREAS.
6. STAGE 2 CANNOT BEGIN UNTIL AFTER ADM SCHOOL BEGINS ON AUGUST 23, 2018.
7. STAGE 2 MAY BE CONSTRUCTED CONCURRENTLY WITH STAGE 1.

<div>DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA</div>		<div>AECOM</div>										DRN																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																			</
--	--	------------------	--	--	--	--	--	--	--	--	--	-----	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	----



DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		DATE		AUGUST, 2018	
		PROJECT NO		60579114	
		FILENAME			
		SHEET NO			
AUDITOR'S PARKING LOT REMOVALS		DRAWING NO		6	
		NO			
		REVISIONS			
		DRN		CHK	
AECOM All Rights Reserved		APP			
		DES			
		DRN			
		Copyright ©			





N 10TH STREET

DEPARTMENT OF  
MOTOR VEHICLES

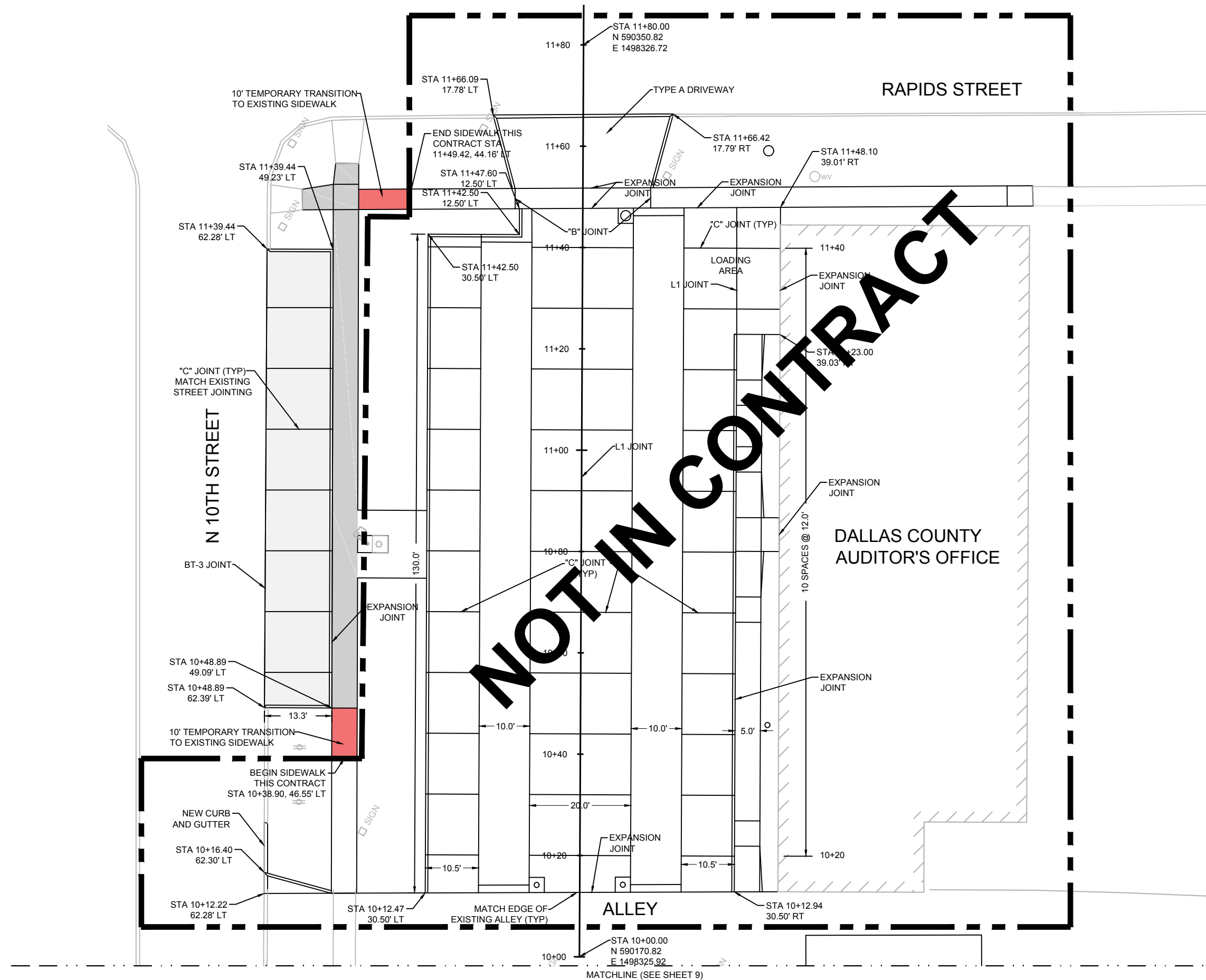
DALLAS COUNTY  
ADMINISTRATION  
BUILDING

COURT STREET

- LEGEND
- HMA PAVEMENT REMOVAL
  - SIDEWALK REMOVAL
  - DRIVEWAY REMOVAL
  - WATER
  - FIBER
  - GAS

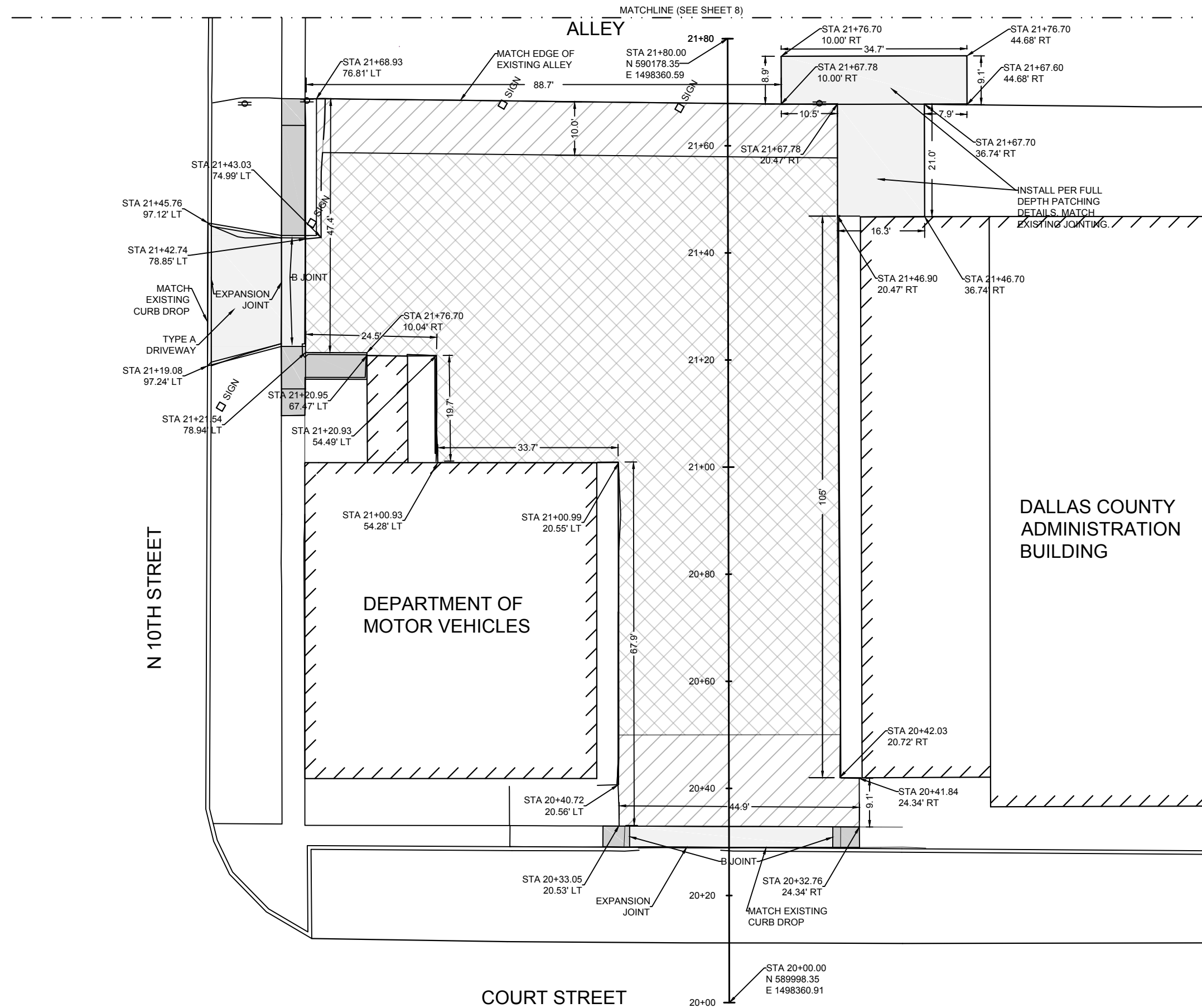


DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		<div>AECOM</div>										Copyright © _____ AECOM All Rights Reserved				NO		REVISIONS		DRN	CHK	DATE
												DRN	DES	CHK	APP							
DATE		AUGUST, 2018																				
PROJECT NO		60579114																				
FILENAME																						
SHEET NO																						
DRAWING NO																						
7																						



LEGEND	
	PCC PAVEMENT
	PERMEABLE PAVERS
	SIDEWALK
	HMA OVERLAY
	RIVER ROCK LANDSCAPING

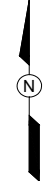
<div>DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA</div>		<div>AUDITOR'S PARKING LOT PROPOSED LAYOUT</div>		<div>AECOM</div>										<div>DRN</div>		<div>DES</div>		<div>CHK</div>		<div>APP</div>		<div>Copyright © AECOM All Rights Reserved</div>		<div>NO</div>		<div>REVISIONS</div>		<div>DRN</div>		<div>CHK</div>		<div>DATE</div>	



NOTE:  
OVERLAY TO BE VARIABLE THICKNESS TO  
IMPROVE DRAINAGE.

**LEGEND**

	PCC PAVEMENT
	PERMEABLE PAVERS
	SIDEWALK
	HMA OVERLAY
	FULL DEPTH HMA PATCH



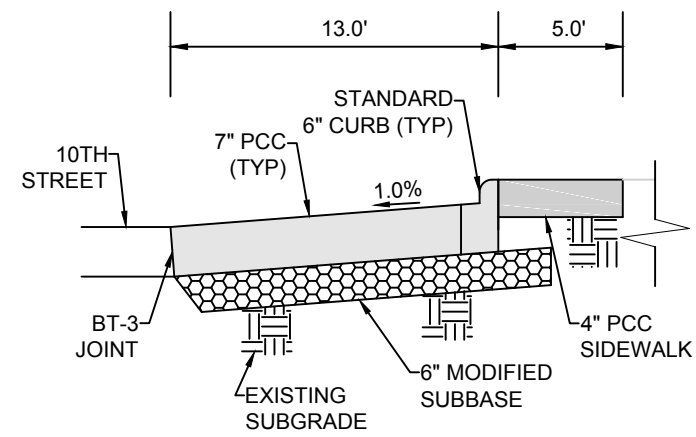
DATE		AUGUST, 2018	
PROJECT NO		60579114	
FILENAME			
SHEET NO			
DRAWING NO		9	

DRN	DES	CHK	APP	Copyright © AECOM All Rights Reserved
NO				
REVISIONS				
DRN	CHK	DATE		

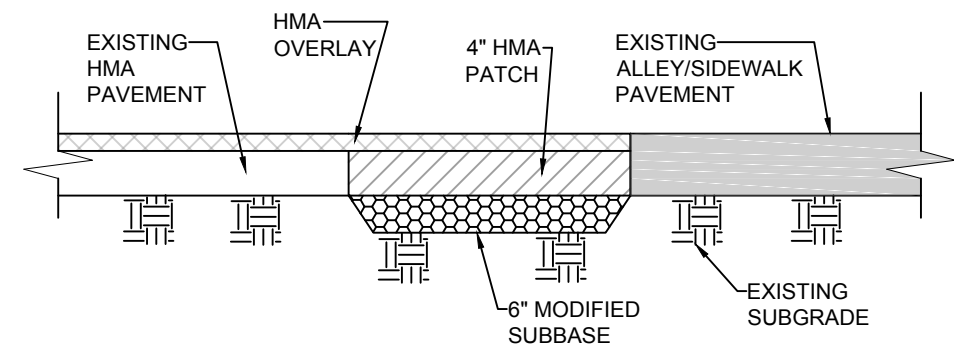
**AECOM**

DALLAS COUNTY  
PARKING LOT REHABILITATION - PHASE 1  
ADEL, IOWA

DMV PARKING LOT  
PROPOSED LAYOUT

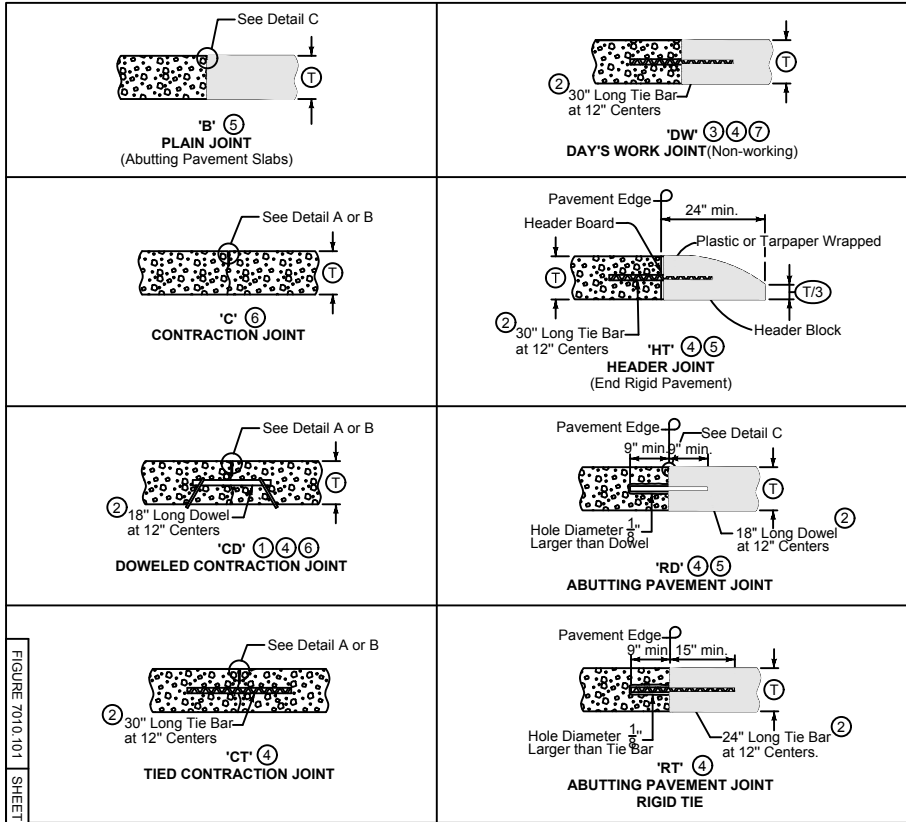


## PROPOSED STREET PARKING

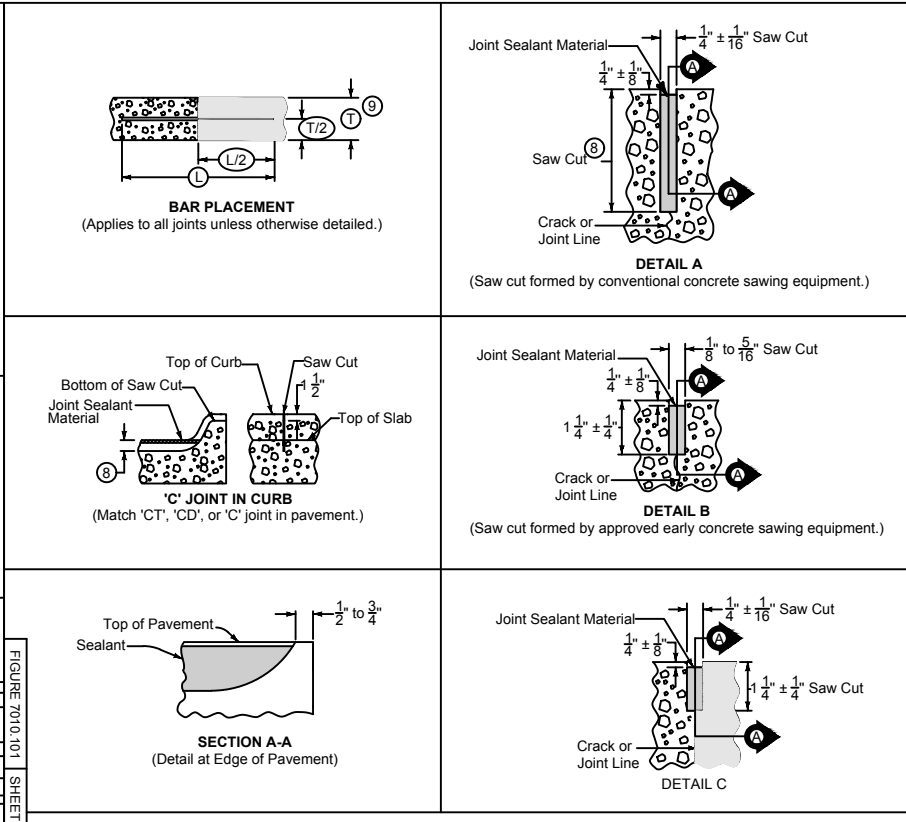
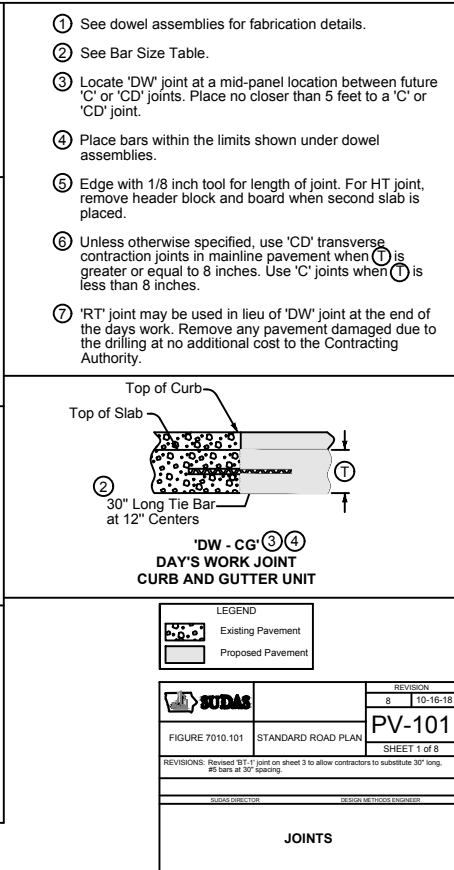


# PROPOSED FULL DEPTH ASPHALT PATCH NTS

<div>DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA</div> <div>TYPICAL SECTION</div>		<div>AECOM</div> <div>Copyright © AECOM All Rights Reserved</div>																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																															
---	--	---	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--



TRANSVERSE CONTRACTION



TRANSVERSE CONTRACTION

BAR SIZE TABLE

①	Dowel Diameter	Tie Bar Size
< 8"	3/4"	#6
≥ 8" but < 10"	1 1/4"	#10
≥ 10"	1 1/2"	#11

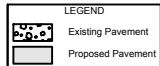
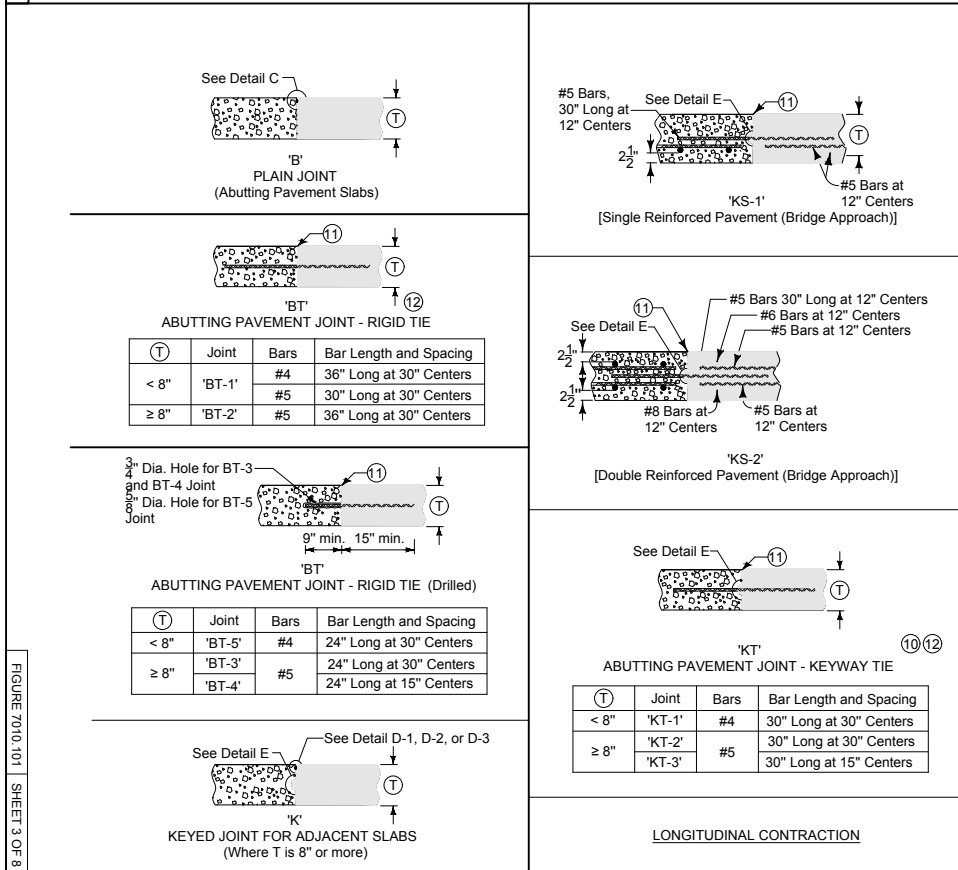


FIGURE 7010.101 STANDARD ROAD PLAN  
PV-101  
SHEET 2 of 8  
REVISIONS: Revised BT-1 joint on sheet 3 to allow contractors to substitute 30" long, #5 bars at 30" spacing.

JOINTS



LONGITUDINAL CONTRACTION

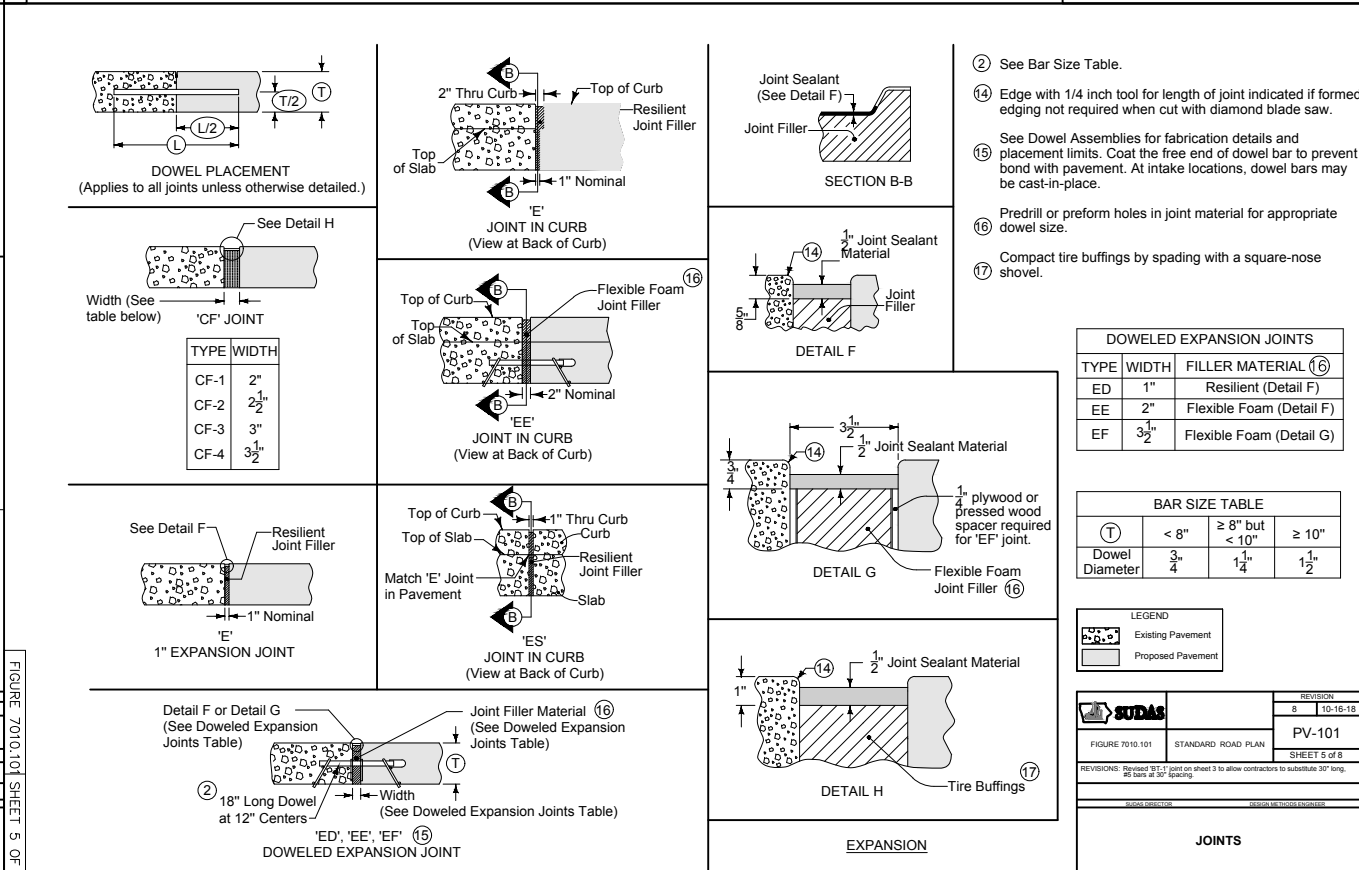
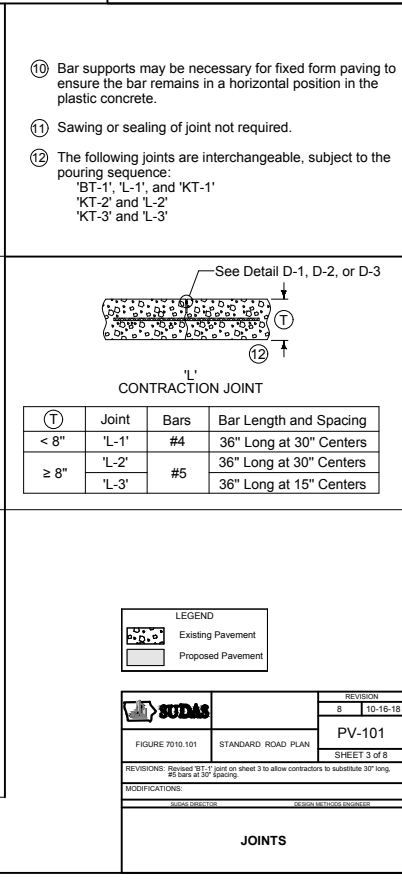
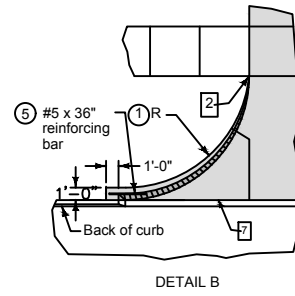
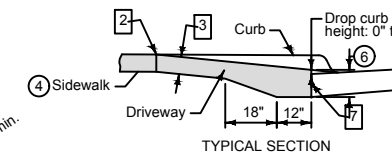
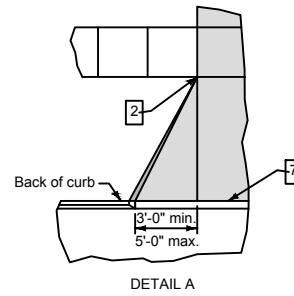
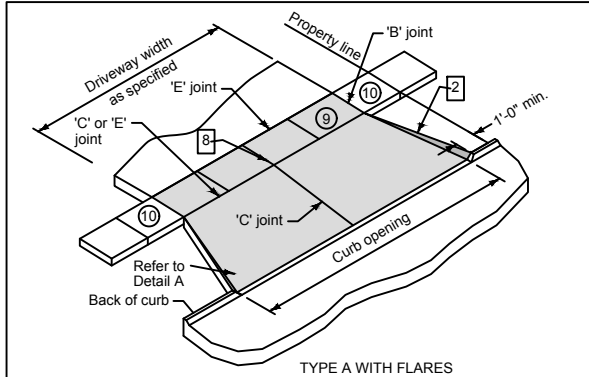


FIGURE 7010.101 SHEET 1 OF 8

FIGURE 7010.101 SHEET 2 OF 8

FIGURE 7010.101 SHEET 3 OF 8

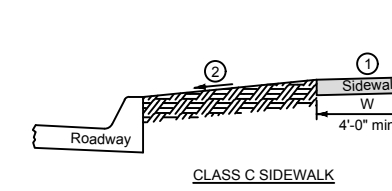
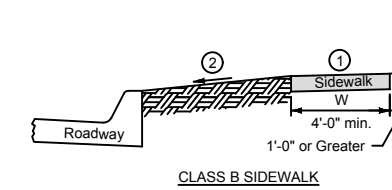
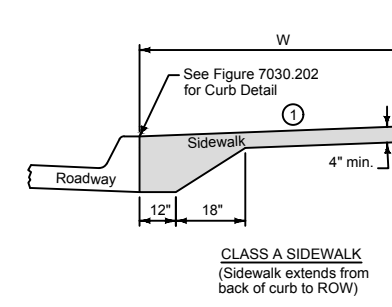
FIGURE 7010.101 SHEET 5 OF 8



- Driveway radius (R). Residential: 10 foot minimum, 15 foot maximum. Commercial and industrial: As specified in the contract documents.
- Transition the curb height to 0 inches at end of taper/radius or at the front edge of sidewalk. Do not extend raised curb across sidewalk.
- Pavement thickness. Residential: 6 inches minimum. Commercial and industrial: 7 inches minimum.
- Sidewalk thickness through driveway to match thickness of driveway.
- Center reinforcing bar vertically in the pavement.
- Match thickness of adjacent roadway, 8 inches minimum.
- Provide 'E' joint at back of curb unless 'B' joint is specified.
- For alleys, invert the pavement crown 2% toward center of alley.
- Target cross slope of 1.5% with a maximum cross slope of 2.0%. If specified in the contract documents, construct the sidewalk through the driveway 5 feet wide to serve as a passing space.
- If cross slope of adjacent sidewalk panel exceeds 2.0%, remove and replace to transition from existing sidewalk to sidewalk through driveway. If elevation change requires a curb ramp, comply with Figure 7030.205; verify need for detectable warning panel with Engineer.

	REVISION
2	10-20-15
7030.101	
SUDAS Standard Specifications	
CONCRETE DRIVEWAY, TYPE A	

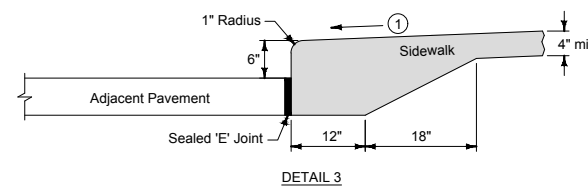
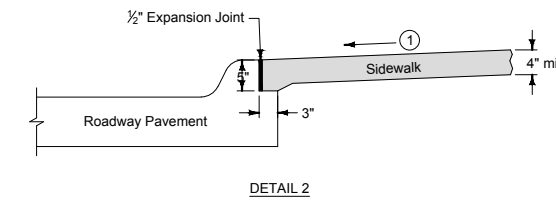
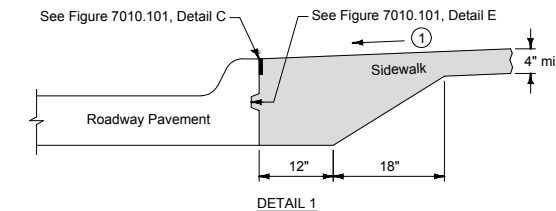
FIGURE 7030.201 SHEET 1 OF 1



- Target cross slope of 1.5% with a maximum cross slope of 2.0% (including sidewalk through driveway).
- Parking Slopes: If parking width is less than 10 feet wide, slope at  $\frac{1}{4}$  inch per foot. If parking width is 10 feet wide and greater, slope at  $\frac{1}{2}$  inch per foot. Special grade may be specified in the contract documents. W = Sidewalk width as specified in the contract documents.

	REVISION
2	10-20-15
7030.201	
SUDAS Standard Specifications	
CLASSES OF SIDEWALKS	

FIGURE 7030.101 SHEET 1 OF 1



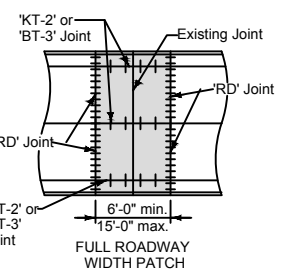
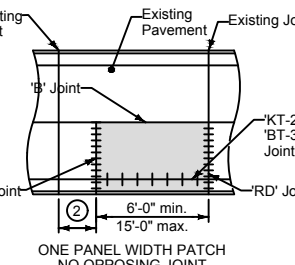
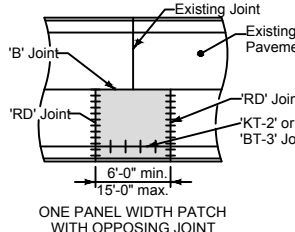
For new sidewalk with new curb and gutter, comply with Detail 1 or Detail 2. Comply with Detail 3 for new sidewalk adjacent to existing pavement or when specified in the contract documents.

- Target cross slope of 1.5% with a maximum cross slope of 2.0%.

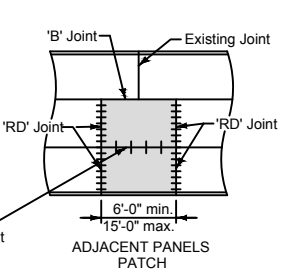
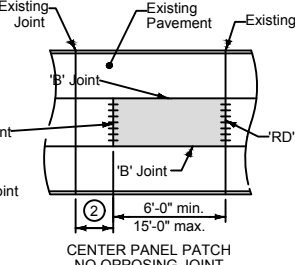
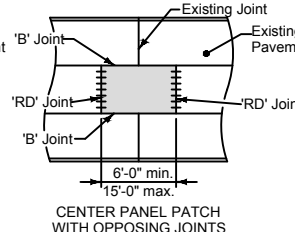
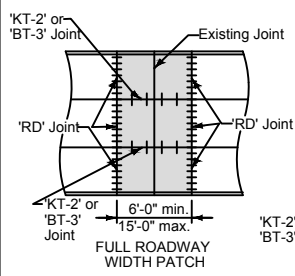
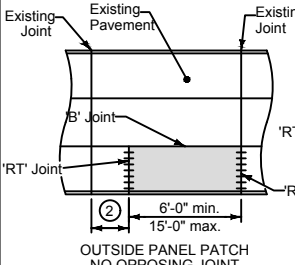
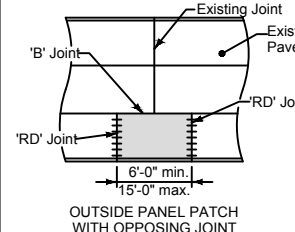
	REVISION
3	10-20-15
7030.202	
SUDAS Standard Specifications	
CURB DETAILS FOR CLASS A SIDEWALK	

FIGURE 7040.101 SHEET 1 OF 1

GUTTERLINE JOINTING

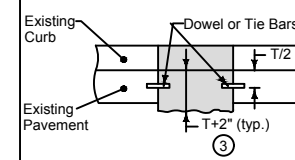


THIRD POINT JOINTING



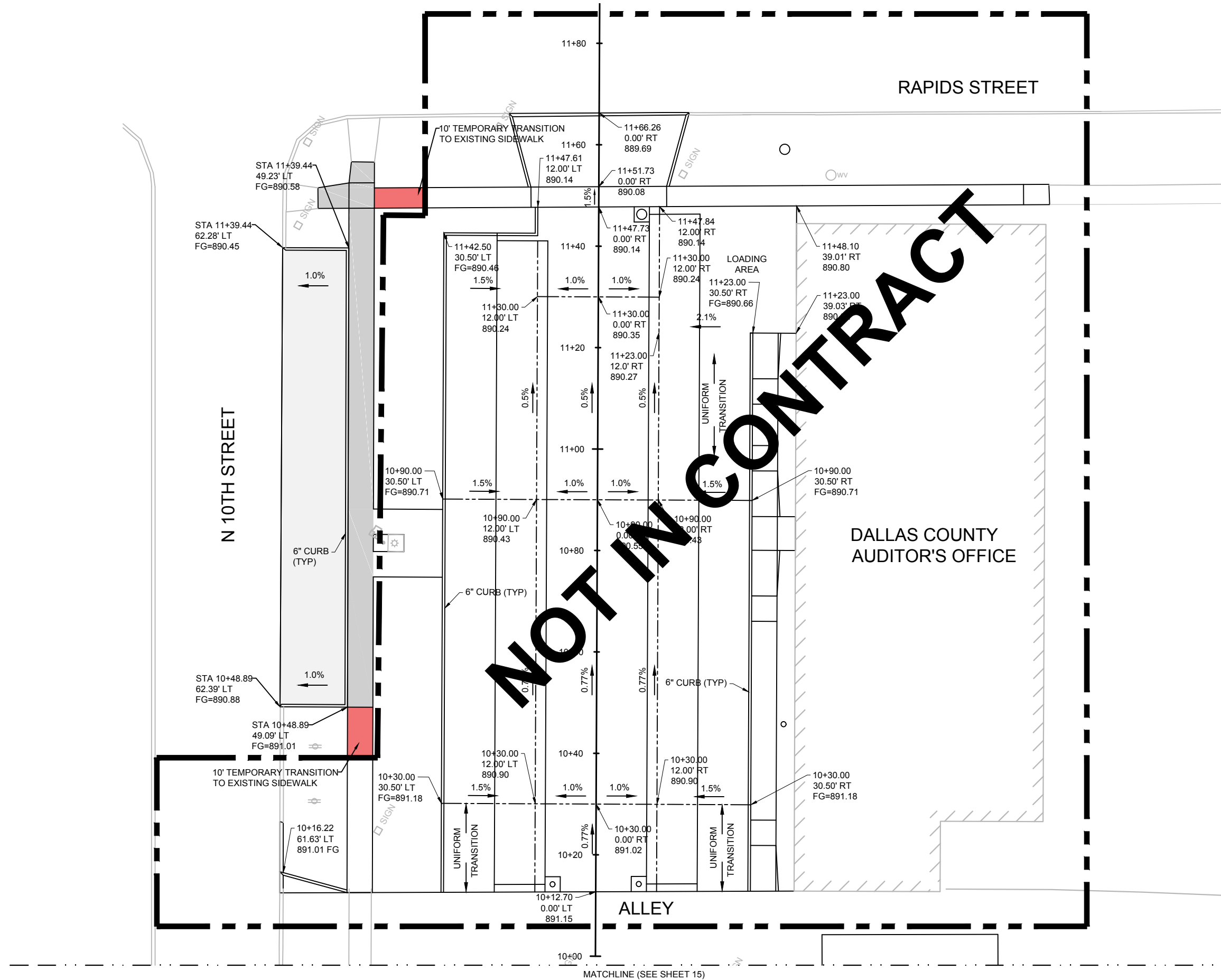
- Patches on roadways with quarter point jointing will be similar to third point jointing details.
- Minimum distance between existing joint and patch is 6 feet. If distance is less than 6 feet, extend patch to existing joint.
- If subgrade or subbase material is required below patch, bring material to a level 2 inches below bottom of existing pavement.

LONGITUDINAL SECTION THRU PCC PATCH

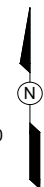


	REVISION
2	10-17-17
7040.101	
SUDAS Standard Specifications	
FULL DEPTH PCC PATCHES LESS THAN OR EQUAL TO 15' LONG	

FIGURE 7030.202 SHEET 1 OF 1

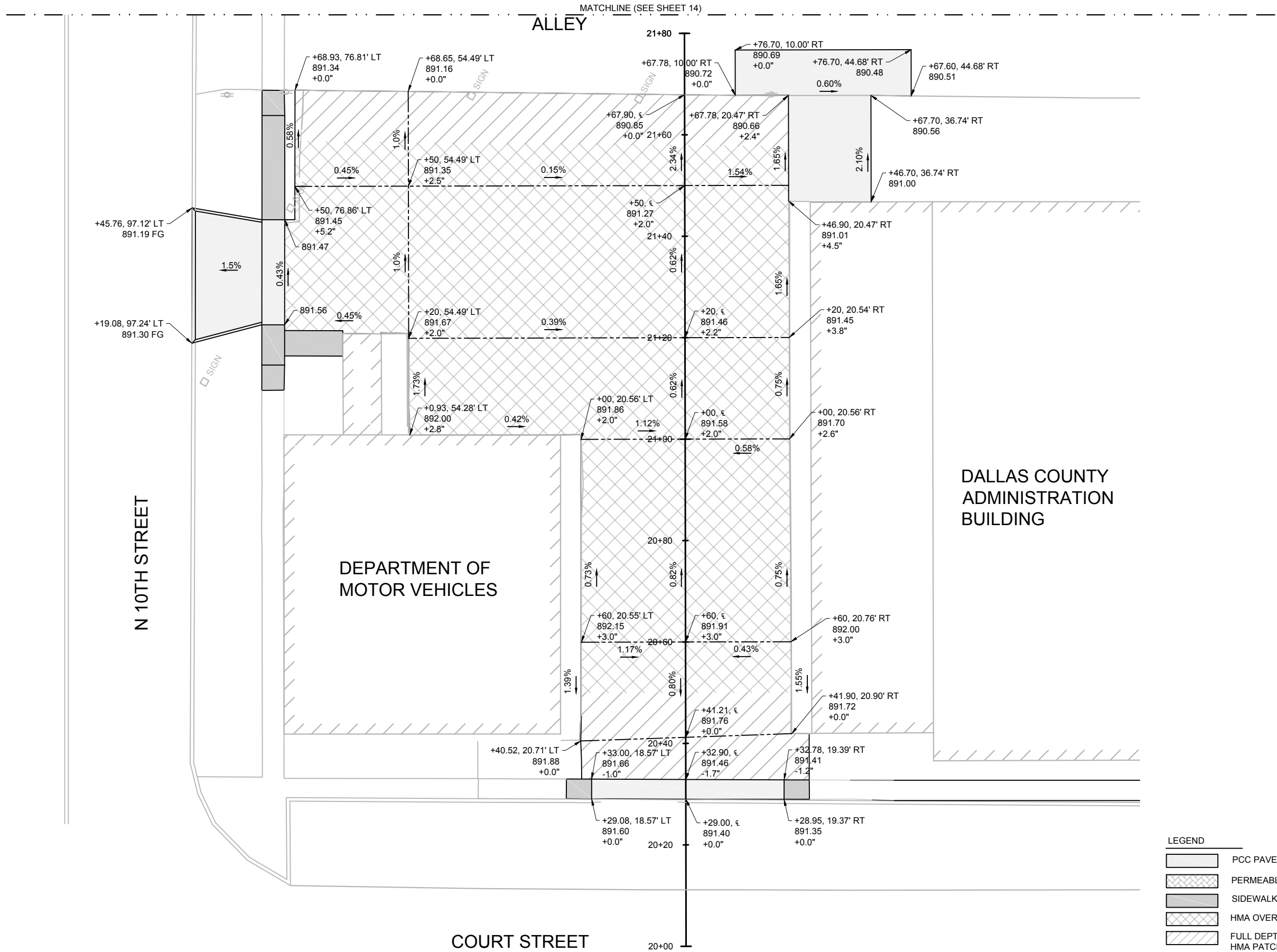


LEGEND	
	PCC PAVEMENT
	PERMEABLE PAVERS
	SIDEWALK
	HMA OVERLAY
	RIVER ROCK LANDSCAPING



NOT IN CONTRACT

DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		AUDITOR'S PARKING LOT PROPOSED GRADING	
DATE	AUGUST, 2018	DRN	CHK
PROJECT NO	60579114	DES	APP
FILENAME		CHK	NO
SHEET NO		APP	NO
DRAWING NO	13	NO	NO
		REVISIONS	
		DRN	CHK
		DES	APP
		CHK	NO
		APP	NO
		Copyright © AECOM All Rights Reserved	

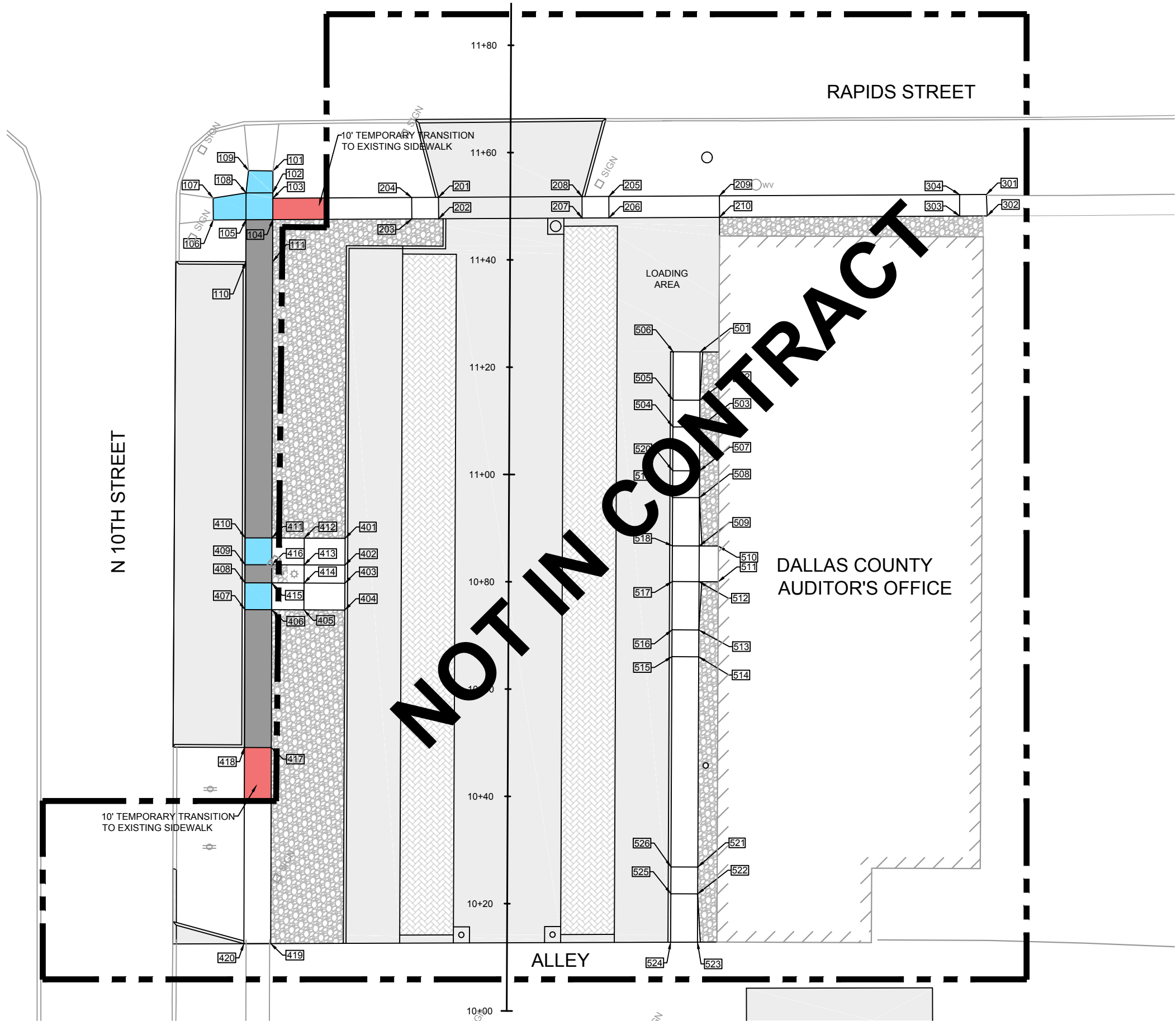


LEGEND	
	PCC PAVEMENT
	PERMEABLE PAVERS
	SIDEWALK
	HMA OVERLAY
	FULL DEPTH HMA PATCH



DATE		AUGUST, 2018
PROJECT NO		60579114
FILENAME		
SHEET NO		
DRAWING NO		14
DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		
DMV PARKING LOT PROPOSED GRADING		
NO		REVISIONS
DRN		CHK
DES		APP
Copyright © AECOM All Rights Reserved		DATE





- LEGEND
- STANDARD SIDEWALK
  - SIDEWALK LANDING
  - SIDEWALK RAMP
  - TEMPORARY SIDEWALK



DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA				AUDITORS PARKING LOT - SIDEWALK			
DATE	AUGUST, 2018			DRN	DES	CHK	APP
PROJECT NO	60579114			Copyright ©	AECOM All Rights Reserved		
FILENAME				NO	REVISIONS		
SHEET NO				DRN	CHK	DATE	
DRAWING NO	15						

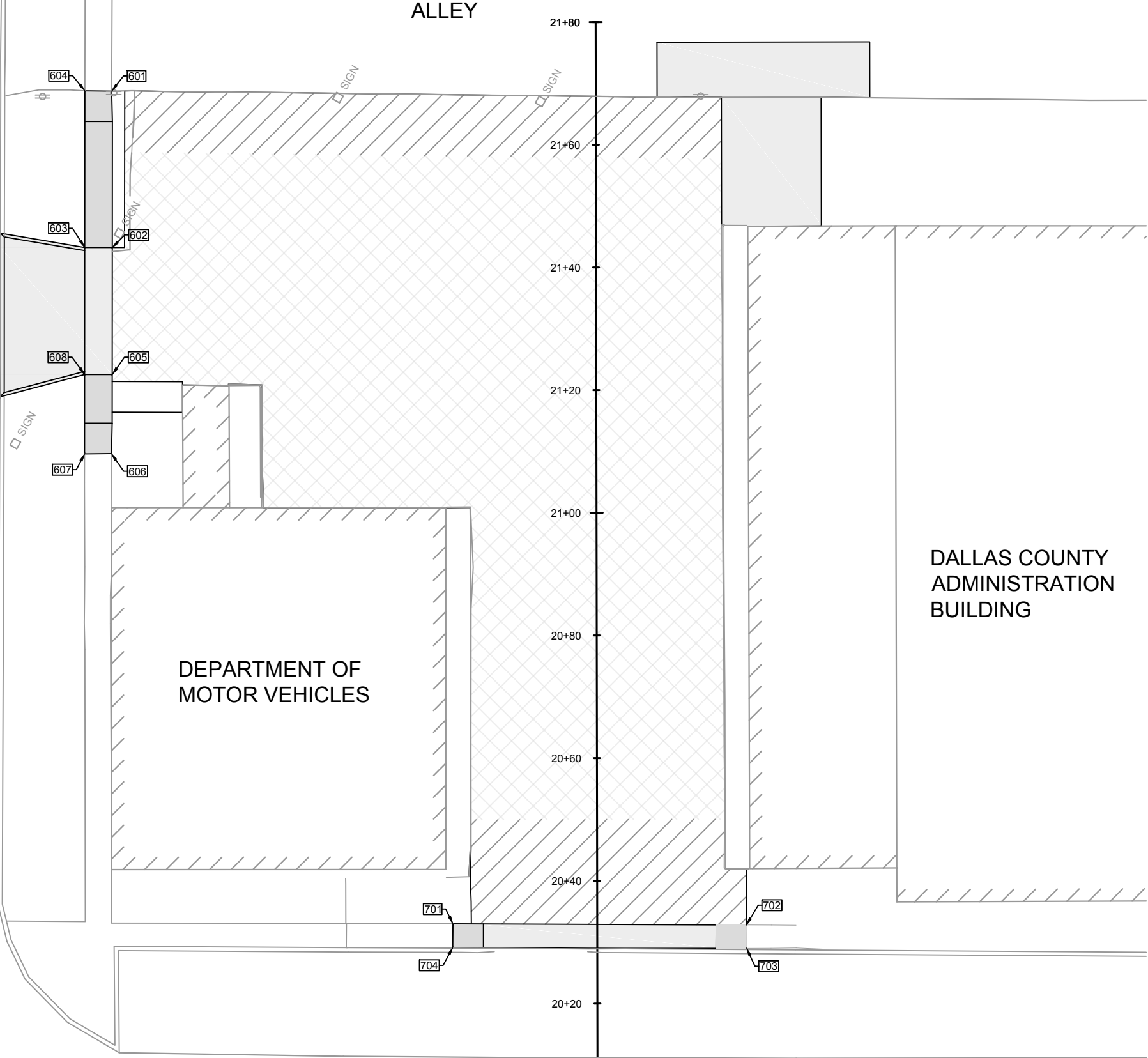
N 10TH STREET

ALLEY

COURT STREET

DEPARTMENT OF  
MOTOR VEHICLES

DALLAS COUNTY  
ADMINISTRATION  
BUILDING



DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		DATE		AUGUST, 2018	
		PROJECT NO		60579114	
		FILENAME			
		SHEET NO			
SIDEWALK		DRAWING NO		16	
		NO		REVISIONS	
		DRN		CHK	
		DES		APP	
AECOM		Copyright ©		AECOM All Rights Reserved	
		DRN		DES	
		CHK		APP	
		SIGN		DATE	

113-10 4-18-2017															
SIDEWALK COMPLIANCE															
See S Sheets															
* Does not include curb Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.															
Point to Point		Sidewalk Designation	_ " PCC Sidewalk	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant?	Measured Slope	Initials	Remarks	FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
				FT	FT	%	Pos. or Neg.		%			Point	Station	Offset	Elevation
101	102	Sidewalk Running Slope	4	4.08	-0.02	-0.5%	0.5% to 5.0%					101	11+56.37	44.26	890.78
101	109	Match Existing Cross Slope	4	4.54	-0.01	-0.2%	Match Existing					102	11+52.30	44.25	890.76
102	103	Landing/Turning Space	4	1.00	0.01	1.0%	0.1% to 2.0%					103	11+51.30	44.25	890.77
102	108	Landing/Turning Space	4	5.00	0.07	1.4%	0.1% to 2.0%					104	11+47.30	44.25	890.83
103	104	Landing/Turning Space	4	4.00	0.06	1.5%	0.1% to 2.0%					105	11+47.25	49.25	890.91
104	105	Landing/Turning Space	4	5.00	0.08	1.6%	0.1% to 2.0%	Yes	Yes			106	11+47.19	55.34	890.92
105	106	Sidewalk Running Slope	4	6.09	0.01	0.2%	0.5% to 5.0%					107	11+51.26	55.40	890.97
105	108	Landing/Turning Space	4	5.00	-0.08	-1.6%	0.1% to 2.0%	Yes	Yes			108	11+52.25	49.25	890.83
106	107	Match Existing Cross Slope	4	4.07	0.05	1.2%	Match Existing					109	11+56.43	48.80	890.77
107	108	Sidewalk Running Slope	4	6.03	-0.14	-2.3%	0.5% to 5.0%					110	11+39.44	49.23	891.08
108	109	Sidewalk Running Slope	4	4.20	-0.06	-1.4%	0.5% to 5.0%					111	11+39.44	44.23	891.16
110	111	Sidewalk Cross Slope	4	5.00	0.08	1.5%	0.5% to 2.0%								
110	105	Sidewalk Running Slope	4	7.81	-0.17	-2.2%	0.5% to 5.0%								
111	104	Sidewalk Running Slope	4	7.81	-0.33	-4.2%	0.5% to 5.0%	Yes	Yes						
201	202	Landing/Turning Space	4	4.22	0.06	1.4%	0.1% to 2.0%					201	11+51.60	13.38	890.08
201	204	Landing/Turning Space	4	5.00	0.07	1.4%	0.1% to 2.0%					202	11+47.60	13.34	890.14
202	203	Landing/Turning Space	4	6.34	0.07	1.1%	0.1% to 2.0%					203	11+47.55	18.34	890.21
203	204	Landing/Turning Space	4	4.00	-0.06	-1.5%	0.1% to 2.0%					204	11+51.55	18.38	890.15
205	206	Landing/Turning Space	4	4.00	0.06	1.5%	0.1% to 2.0%					205	11+51.90	18.37	890.15
205	208	Landing/Turning Space	4	5.00	-0.07	-1.4%	0.1% to 2.0%					206	11+47.90	18.41	890.21
206	207	Landing/Turning Space	4	6.41	-0.07	-1.1%	0.1% to 2.0%					207	11+47.86	13.41	890.14
207	208	Landing/Turning Space	4	4.24	-0.06	-1.4%	0.1% to 2.0%					208	11+51.86	13.37	890.08
209	205	Sidewalk Running Slope	4	20.61	-0.59	-2.9%	0.5% to 5.0%					209	11+52.10	38.97	890.74
209	210	Sidewalk Cross Slope	4	4.00	0.06	1.5%	0.5% to 2.0%					210	11+48.10	39.01	890.80
210	206	Sidewalk Running Slope	4	20.61	-0.59	-2.9%	0.5% to 5.0%								
301	302	Match Existing Cross Slope	4	3.97	0.11	2.8%	Match Existing					301	11+52.59	88.73	890.11
301	304	Landing/Turning Space	4	5.00	0.08	1.6%	0.1% to 2.0%	Yes	Yes			302	11+48.59	88.90	890.22
302	303	Landing/Turning Space	4	5.13	0.03	0.6%	0.1% to 2.0%					303	11+48.54	83.77	890.25
303	304	Landing/Turning Space	4	4.00	-0.06	-1.5%	0.1% to 2.0%					304	11+52.54	83.73	890.19
401	402	Landing/Turning Space	4	4.99	0.03	0.7%	0.1% to 2.0%					401	10+87.96	30.50	891.23
401	412	Landing/Turning Space	4	7.61	0.10	1.4%	0.1% to 2.0%					402	10+82.96	30.50	891.26
402	403	Landing/Turning Space	4	3.37	0.03	0.9%	0.1% to 2.0%					403	10+79.59	30.50	891.29
402	413	Landing/Turning Space	4	7.62	0.14	1.8%	0.1% to 2.0%	Yes	Yes			404	10+74.58	30.50	891.33
403	404	Landing/Turning Space	4	5.01	0.04	0.8%	0.1% to 2.0%					405	10+74.59	38.13	891.41
403	414	Landing/Turning Space	4	7.62	0.09	1.2%	0.1% to 2.0%					406	10+74.60	44.13	891.47
404	405	Landing/Turning Space	4	7.63	0.08	1.0%	0.1% to 2.0%					407	10+74.59	49.13	891.39
405	406	Sidewalk Running Slope	4	6.01	0.06	0.9%	0.5% to 5.0%					408	10+79.59	49.14	891.37
405	414	Landing/Turning Space	4	5.00	-0.03	-0.6%	0.1% to 2.0%					409	10+82.96	49.14	891.36
406	407	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					410	10+87.96	49.15	891.33
406	415	Landing/Turning Space	4	5.00	-0.02	-0.4%	0.1% to 2.0%					411	10+87.97	44.15	891.41
407	408	Landing/Turning Space	4	5.00	-0.02	-0.4%	0.1% to 2.0%					412	10+87.96	38.11	891.33
408	409	Sidewalk Running Slope	4	3.37	-0.01	-0.3%	0.5% to 5.0%					413	10+82.96	38.12	891.40
408	415	Landing/Turning Space	4	5.00	0.08	1.5%	0.1% to 2.0%					414	10+79.59	38.15	891.38
409	410	Landing/Turning Space	4	5.00	-0.03	-0.6%	0.1% to 2.0%					415	10+79.60	44.14	891.45
409	416	Landing/Turning Space	4	5.00	0.08	1.5%	0.1% to 2.0%					416	10+82.97	44.14	891.44
410	411	Landing/Turning Space	4	5.00	0.08	1.5%	0.1% to 2.0%					417	10+48.90	44.09	891.59
411	412	Sidewalk Running Slope	4	6.04	-0.08	-1.2%	0.5% to 5.0%					418	10+48.89	49.09	891.51
411	416	Landing/Turning Space	4	5.00	0.03	0.6%	0.1% to 2.0%					419	10+12.36	44.03	891.29
412	413	Landing/Turning Space	4	5.00	0.07	1.4%	0.1% to 2.0%					420	10+12.32	49.03	891.26
413	414	Landing/Turning Space	4	3.37	-0.02	-0.6%	0.1% to 2.0%								
413	416	Sidewalk Running Slope	4	6.03	0.04	0.6%	0.5% to 5.0%								
414	415	Sidewalk Running Slope	4	6.02	0.07	1.1%	0.5% to 5.0%								
415	416	Sidewalk Running Slope	4	3.37	-0.01	-0.3%	0.5% to 5.0%								
417	418	Sidewalk Running Slope	4	5.00	-0.08	-1.5%	0.5% to 5.0%								
419	420	Match Existing Cross Slope	4	5.01	-0.03	-0.6%	Match Existing								

DRN

DES

CHK

APP

Copyright ©  
AECOM All Rights Reserved

NO

REVISIONS

DRN

CHK

DATE

DALLAS COUNTY  
PARKING LOT REHABILITATION - PHASE 1  
ADEL, IOWA

SIDEWALK

DATE  
AUGUST, 2018

PROJECT NO  
60579114

FILENAME

SHEET NO

DRAWING NO

17



113-10  
4-18-2017

SIDEWALK COMPLIANCE

See S Sheets

\* Does not include curb  
Staking required by Contracting Authority per Article 2511.03 of the Standard Specifications.

												FOR INFORMATION ONLY: VALUES USED TO DETERMINE DESIGNED SLOPES			
Point to Point		Sidewalk Designation	_ " PCC Sidewalk	Distance*	Δ Elevation	Slope	Acceptable Constructed Range	Staking Required on this Quadrant?	Measured Slope	Initials	Remarks	Point	Station	Offset	Elevation
				FT	FT	%	Pos. or Neg.		%						
501	502	Ramp Running Slope	6	9.00	0.50	5.5%	0.5% to 8.3%					501	11+23.00	35.50	890.75
501	506	Ramp Cross Slope	6	5.00	-0.09	-1.8%	0.1% to 2.0%	Yes	Yes			502	11+14.00	35.50	891.25
502	503	Landing/Turning Space	4	5.00	0.01	0.2%	0.1% to 2.0%					503	11+09.00	35.50	891.26
502	505	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					504	11+09.00	30.50	891.18
503	504	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					505	11+14.00	30.50	891.17
503	507	Sidewalk Running Slope	4	8.17	-0.01	-0.1%	0.5% to 5.0%					506	11+23.00	30.50	890.66
504	505	Landing/Turning Space	4	5.00	-0.01	-0.2%	0.1% to 2.0%					507	11+00.83	35.50	891.25
504	520	Sidewalk Running Slope	4	8.17	-0.01	-0.1%	0.5% to 5.0%					508	10+95.83	35.50	891.26
505	506	Ramp Running Slope	6	9.00	-0.51	-5.7%	0.5% to 8.3%					509	10+86.83	35.50	890.82
507	508	Landing/Turning Space	4	5.00	0.01	0.2%	0.1% to 2.0%					510	10+86.86	39.06	890.86
507	520	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					511	10+80.17	39.06	890.88
508	509	Ramp Running Slope	6	9.00	-0.43	-4.8%	0.5% to 8.3%					512	10+80.17	35.50	890.84
508	519	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					513	10+71.17	35.50	891.45
509	510	Landing/Turning Space	4	3.56	0.04	1.1%	0.1% to 2.0%					514	10+66.17	35.50	891.49
509	512	Landing/Turning Space	4	6.66	0.02	0.3%	0.1% to 2.0%					515	10+66.17	30.50	891.41
509	518	Landing/Turning Space	4	5.00	-0.09	-1.8%	0.1% to 2.0%	Yes	Yes			516	10+71.17	30.50	891.37
510	511	Landing/Turning Space	4	6.66	0.02	0.3%	0.1% to 2.0%					517	10+80.17	30.50	890.75
511	512	Landing/Turning Space	4	3.57	-0.04	-1.1%	0.1% to 2.0%					518	10+86.83	30.50	890.73
512	513	Ramp Running Slope	6	9.00	0.61	6.7%	0.5% to 8.3%					519	10+95.83	30.50	891.18
512	517	Landing/Turning Space	4	5.00	-0.09	-1.8%	0.1% to 2.0%	Yes	Yes			520	11+00.83	30.50	891.17
513	514	Landing/Turning Space	4	5.00	0.04	0.8%	0.1% to 2.0%					521	10+26.98	35.50	891.78
513	516	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					522	10+21.98	35.50	891.82
514	515	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%					523	10+12.98	35.50	891.35
514	521	Sidewalk Running Slope	4	39.19	0.29	0.7%	0.5% to 5.0%					524	10+12.94	30.50	891.31
515	516	Landing/Turning Space	4	5.00	-0.04	-0.8%	0.1% to 2.0%					525	10+21.98	30.50	891.74
515	526	Sidewalk Running Slope	4	39.19	0.29	0.7%	0.5% to 5.0%					526	10+26.98	30.50	891.70
516	517	Ramp Running Slope	6	9.00	-0.62	-6.9%	0.5% to 8.3%								
517	518	Landing/Turning Space	4	6.66	-0.02	-0.3%	0.1% to 2.0%								
518	519	Ramp Running Slope	6	9.00	0.45	5.0%	0.5% to 8.3%								
519	520	Landing/Turning Space	4	5.00	-0.01	-0.2%	0.1% to 2.0%								
521	522	Landing/Turning Space	4	5.00	0.04	0.8%	0.1% to 2.0%								
521	526	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%								
522	523	Ramp Running Slope	6	9.00	-0.47	-5.2%	0.5% to 8.3%								
522	525	Landing/Turning Space	4	5.00	-0.08	-1.5%	0.1% to 2.0%								
523	524	Match Existing Cross Slope	4	5.00	-0.04	-0.8%	Match Existing								
524	525	Ramp Running Slope	6	9.04	0.43	4.8%	0.5% to 8.3%								
525	526	Landing/Turning Space	4	5.00	-0.04	-0.8%	0.1% to 2.0%								
601	604	Match Existing Cross Slope	4	4.37	0.01	0.2%	Match Existing					601	21+68.90	78.94	891.36
601	602	Sidewalk Running Slope	4	25.51	0.11	0.4%	0.5% to 5.0%					602	21+43.39	78.88	891.47
602	603	Sidewalk Cross Slope	4	4.50	-0.07	-1.5%	0.5% to 2.0%					603	21+43.40	83.38	891.40
603	604	Sidewalk Running Slope	4	25.56	-0.03	-0.1%	0.5% to 5.0%					604	21+68.96	83.31	891.37
605	608	Sidewalk Cross Slope	4	4.50	-0.07	-1.5%	0.5% to 2.0%					605	21+22.67	78.93	891.56
605	606	Sidewalk Running Slope	4	12.87	-0.03	-0.2%	0.5% to 5.0%					606	21+09.81	79.09	891.53
606	607	Match Existing Cross Slope	4	4.38	-0.14	-3.2%	Match Existing					607	21+09.76	83.46	891.39
607	608	Sidewalk Running Slope	4	12.93	0.10	0.8%	0.5% to 5.0%					608	21+22.68	83.43	891.49
701	704	Match Existing Cross Slope	4	3.87	-0.19	-4.9%	Match Existing					701	20+33.03	23.53	891.82
702	703	Match Existing Cross Slope	4	3.81	-0.13	-3.4%	Match Existing					702	20+32.76	24.34	891.46
												703	20+28.95	24.37	891.33
												704	20+29.16	23.57	891.63

DRN

DES

CHK

APP

Copyright ©  
AECOM All Rights Reserved

NO

REVISIONS

DRN

CHK

DATE

DALLAS COUNTY  
PARKING LOT REHABILITATION - PHASE 1  
ADEL, IOWA

SIDEWALK

DATE  
AUGUST, 2018

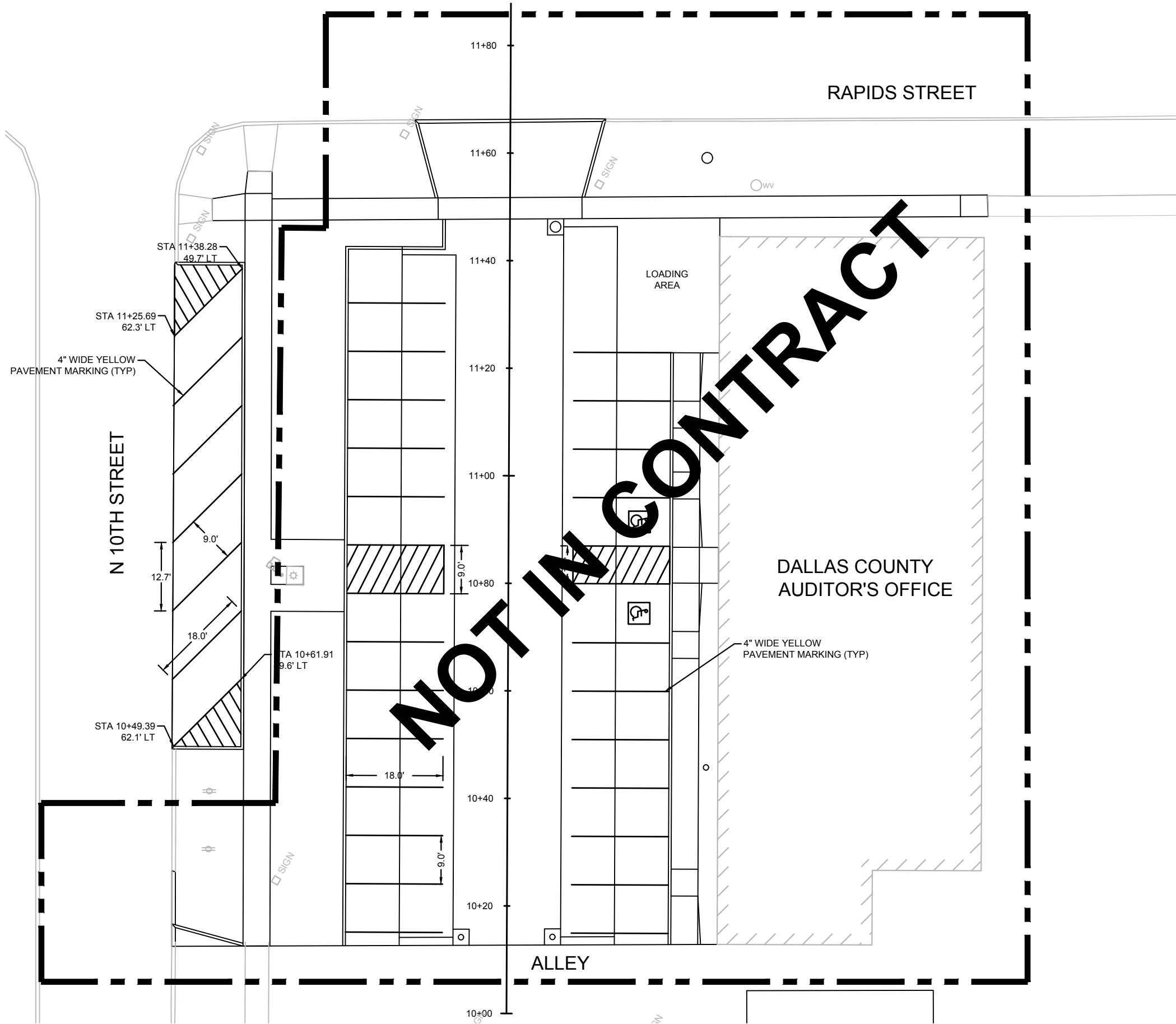
PROJECT NO  
60579114

FILENAME

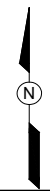
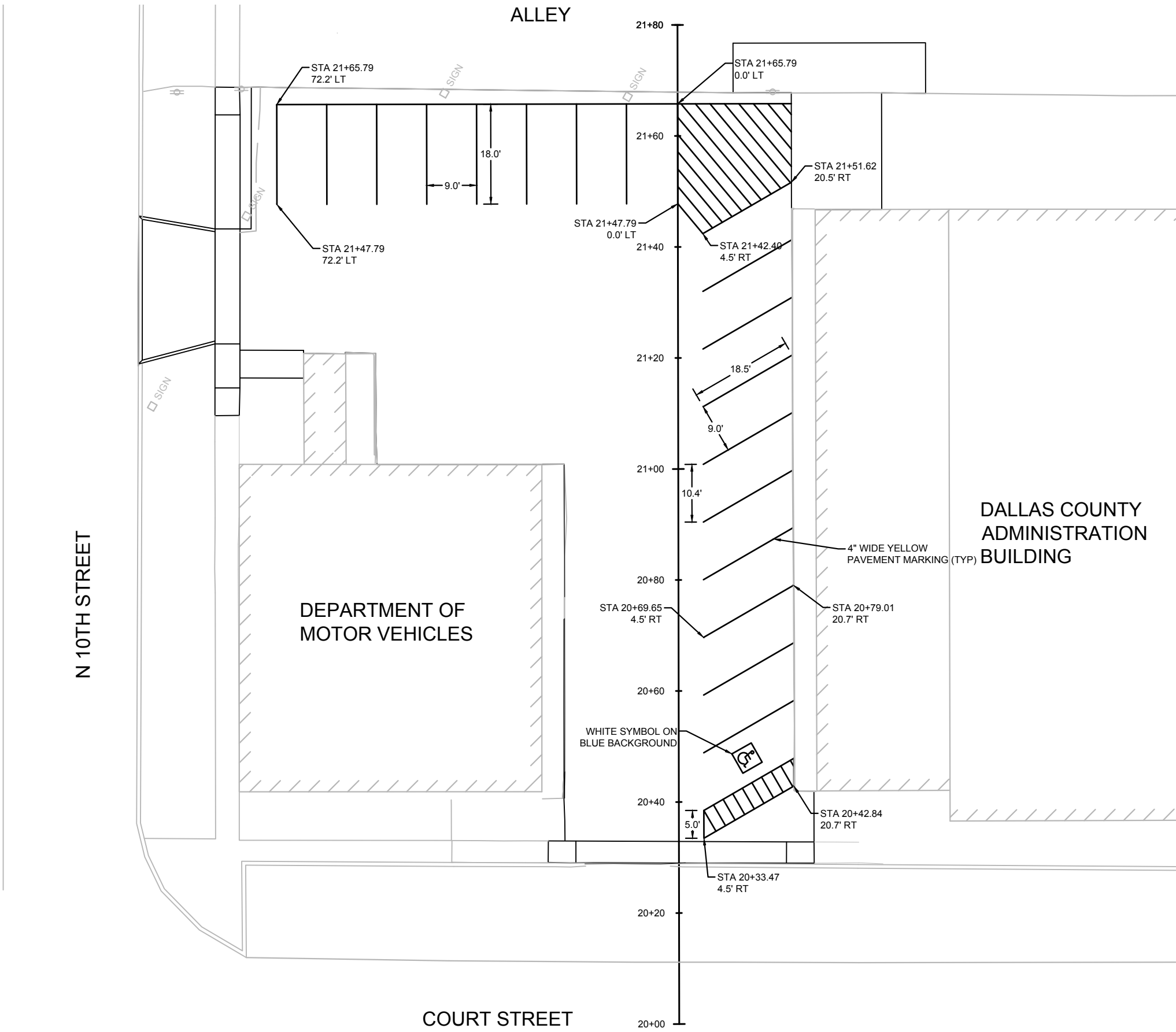
SHEET NO

DRAWING NO

18



DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		DATE				AUGUST, 2018			
		PROJECT NO				60579114			
		FILENAME							
		SHEET NO							
PERMANENT MARKING		DRAWING NO				19			
		NO				REVISIONS			
		DRN				CHK			
		DES				APP			
AECOM		Copyright ©				AECOM All Rights Reserved			
		DRN				DES			
		CHK				APP			
		DES				APP			



DALLAS COUNTY PARKING LOT REHABILITATION - PHASE 1 ADEL, IOWA		AECOM		Copyright © _____ AECOM All Rights Reserved		DRN																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																															
---	--	-------	--	--	--	-----	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--